

HUMBOLDT COUNTY DEPARTMENT OF PUBLIC WORKS  
ROAD EVALUATION REPORT

APR 11 11963



**PART A:** *Part A may be completed by the applicant*

Applicant Name: Eshea Bennett

APN: 523-021-003

Planning & Building Department Case/File No.: \_\_\_\_\_

Road Name: Horse Linto Road *(complete a separate form for each road)*

From Road (Cross street): Patterson Lane

To Road (Cross street): River Access Road / FS #7N02

Length of road segment: .9 miles Date Inspected: \_\_\_\_\_

Road is maintained by:  County  Other US Forest Service  
(State, Forest Service, National Park, State Park, BLM, Private, Tribal, etc)

Check one of the following:

Box 1  The entire road segment is developed to Category 4 road standards (20 feet wide) or better. If checked, then the road is adequate for the proposed use without further review by the applicant.

Box 2  The entire road segment is developed to the equivalent of a road category 4 standard. If checked, then the road is adequate for the proposed use without further review by the applicant.

*An equivalent road category 4 standard is defined as a roadway that is generally 20 feet in width, but has pinch points which narrow the road. Pinch points include, but are not limited to, one-lane bridges, trees, large rock outcroppings, culverts, etc. Pinch points must provide visibility where a driver can see oncoming vehicles through the pinch point which allows the oncoming vehicle to stop and wait in a 20 foot wide section of the road for the other vehicle to pass.*

Box 3  The entire road segment is not developed to the equivalent of road category 4 or better. The road may or may not be able to accommodate the proposed use and further evaluation is necessary. Part B is to be completed by a Civil Engineer licensed by the State of California.

The statements in PART A are true and correct and have been made by me after personally inspecting and measuring the road.

Signature Eshea Bennett

Date 7/27/18

Name Printed Eshea Bennett

**Important: Read the instructions before using this form. If you have questions, please call the Dept. of Public Works Land Use Division at 707.445.7285.**

HUMBOLDT COUNTY DEPARTMENT OF PUBLIC WORKS  
ROAD EVALUATION REPORT

APP#  
11963



**PART A:** *Part A may be completed by the applicant*

Applicant Name: Eshea Bennett APN: 523-021-003

Planning & Building Department Case/File No.: \_\_\_\_\_

Road Name: River Access Road / FS # 7N02 (complete a separate form for each road)

From Road (Cross street): Horse Linto Road

To Road (Cross street): Access Driveway

Length of road segment: 1.1 miles Date Inspected: \_\_\_\_\_

Road is maintained by:  County  Other US Forest Service / Private  
(State, Forest Service, National Park, State Park, BLM, Private, Tribal, etc)

Check one of the following:

Box 1  The entire road segment is developed to Category 4 road standards (20 feet wide) or better. If checked, then the road is adequate for the proposed use without further review by the applicant.

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The statements in PART A are true and correct and have been made by me after personally inspecting and measuring the road.

Eshea Bennett  
Signature

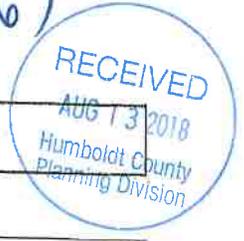
7/27/18  
Date

Eshea Bennett  
Name Printed

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HUMBOLDT COUNTY DEPARTMENT OF PUBLIC WORKS  
ROAD EVALUATION REPORT

#11963



**PART A: Part A may be completed by the applicant**

Applicant Name: Eshea Bennett APN: 523-021-003

Planning & Building Department Case/File No.: \_\_\_\_\_

Road Name: Access Driveway (complete a separate form for each road)

From Road (Cross street): River Access Road / FS # 7N02

To Road (Cross street): Project Site

Length of road segment: .7 miles Date Inspected: 3/14/18

Road is maintained by:  County  Other Private  
(State, Forest Service, National Park, State Park, BLM, Private, Tribal, etc)

Check one of the following:

Box 1  The entire road segment is developed to Category 4 road standards (20 feet wide) or better. If checked, then the road is adequate for the proposed use without further review by the applicant.

Box 2  The entire road segment is developed to the equivalent of a road category 4 standard. If checked, then the road is adequate for the proposed use without further review by the applicant.

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Box 3  The entire road segment is not developed to the equivalent of road category 4 or better. The road may or may not be able to accommodate the proposed use and further evaluation is necessary. Part B is to be completed by a Civil Engineer licensed by the State of California.

The statements in PART A are true and correct and have been made by me after personally inspecting and measuring the road.

Eshea Bennett Signature Date 7/27/18

Eshea Bennett Name Printed

**Important: Read the instructions before using this form. If you have questions, please call the Dept. of Public Works Land Use Division at 707.445.7205.**

**PART B: Only complete Part B if Box 3 is checked in Part A. Part B is to be completed by a Civil Engineer licensed by the State of California. Complete a separate form for each road.**

Road Name: River Access Rd / 7N02 Date Inspected: \_\_\_\_\_ APN: 523-021-003  
 From Road: Horse Linto Road (Post Mile 0.9 ) Planning & Building  
 To Road: Access Driveway (Post Mile 1.1 ) Department Case/File No.:

1. What is the Average Daily Traffic (ADT) of the road (including other known cannabis projects)?

Number of other known cannabis projects included in ADT calculations:  
 (Contact the Planning & Building Department for information on other nearby projects.) 7

ADT: 85 Date(s) measured: \_\_\_\_\_

Method used to measure ADT:  Counters  Estimated using ITE Trip Generation Book

Is the ADT of the road less than 400?  Yes  No

If YES, then the road is considered very low volume and shall comply with the design standards outlined in the American Association of State Highway and Transportation Officials (AASHTO) *Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤400)*. Complete sections 2 and 3 below.

If NO, then the road shall be reviewed per the applicable policies for the design of local roads and streets presented in AASHTO *A Policy on Geometric Design of Highways and Streets*, commonly known as the "Green Book". Complete section 3 below.

2. Identify site specific safety problems with the road that include, but are not limited to: (Refer to Chapter 3 in AASHTO *Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤400)* for guidance.)

A. Pattern of curve related crashes.

Check one:  No.  Yes, see attached sheet for Post Mile (PM) locations.

B. Physical evidence of curve problems such as skid marks, scarred trees, or scarred utility poles

Check one:  No.  Yes, see attached sheet for PM locations.

C. Substantial edge rutting or encroachment.

Check one:  No.  Yes, see attached sheet for PM locations.

D. History of complaints from residents or law enforcement.

Check one:  No.  Yes ( check if written documentation is attached)

E. Measured or known speed substantially higher than the design speed of the road (20+ MPH higher)

Check one:  No.  Yes.

F. Need for turn-outs.

Check one:  No.  Yes, see attached sheet for PM locations.

3. Conclusions/Recommendations per AASHTO. Check one:

The roadway can accommodate the cumulative increased traffic from this project and all known cannabis projects identified above.

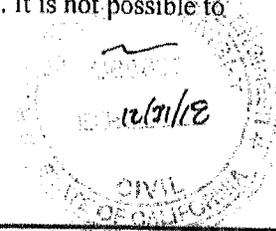
The roadway can accommodate the cumulative increased traffic from this project and all known cannabis projects identified above, if the recommendations on the attached report are done. ( check if a *Neighborhood Traffic Management Plan* is also required and is attached.)

The roadway cannot accommodate increased traffic from the proposed use. It is not possible to address increased traffic.

A map showing the location and limits of the road being evaluated in PART B is attached. The statements in PART B are true and correct and have been made by me after personally evaluating the road.

Signature of Civil Engineer \_\_\_\_\_

Date 7/27/18



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**PART B: Only complete Part B if Box 3 is checked in Part A. Part B is to be completed by a Civil Engineer licensed by the State of California. Complete a separate form for each road.**

Road Name: Access Driveway Date Inspected: 3/14/18 APN: 523-021-003  
 From Road: River Access Rd / 7N02 (Post Mile 1.1 ) Planning & Building  
 To Road: Project Site (Post Mile .7 ) Department Case/File No.:

1. What is the Average Daily Traffic (ADT) of the road (including other known cannabis projects)?

Number of other known cannabis projects included in ADT calculations:  
 (Contact the Planning & Building Department for information on other nearby projects.) 1

ADT: 8 Date(s) measured: 3/14/18

Method used to measure ADT:  Counters  Estimated using ITE Trip Generation Book

Is the ADT of the road less than 400?  Yes  No

If YES, then the road is considered very low volume and shall comply with the design standards outlined in the American Association of State Highway and Transportation Officials (AASHTO) Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤400). Complete sections 2 and 3 below.

If NO, then the road shall be reviewed per the applicable policies for the design of local roads and streets presented in AASHTO A Policy on Geometric Design of Highways and Streets, commonly known as the "Green Book". Complete section 3 below.

2. Identify site specific safety problems with the road that include, but are not limited to: (Refer to Chapter 3 in AASHTO Guidelines for Geometric Design of Very Low-Volume Local Roads (ADT ≤400) for guidance.)

A. Pattern of curve related crashes.

Check one:  No.  Yes, see attached sheet for Post Mile (PM) locations.

B. Physical evidence of curve problems such as skid marks, scarred trees, or scarred utility poles

Check one:  No.  Yes, see attached sheet for PM locations.

C. Substantial edge rutting or encroachment.

Check one:  No.  Yes, see attached sheet for PM locations.

D. History of complaints from residents or law enforcement.

Check one:  No.  Yes ( check if written documentation is attached)

E. Measured or known speed substantially higher than the design speed of the road (20+ MPH higher)

Check one:  No.  Yes.

F. Need for turn-outs.

Check one:  No.  Yes, see attached sheet for PM locations.

3. Conclusions/Recommendations per AASHTO. Check one:

The roadway can accommodate the cumulative increased traffic from this project and all known cannabis projects identified above.

The roadway can accommodate the cumulative increased traffic from this project and all known cannabis projects identified above, if the recommendations on the attached report are done. ( check if a Neighborhood Traffic Management Plan is also required and is attached.)

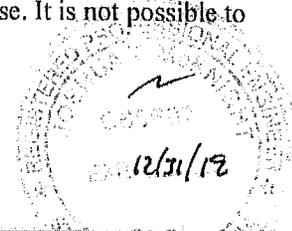
The roadway cannot accommodate increased traffic from the proposed use. It is not possible to address increased traffic.

A map showing the location and limits of the road being evaluated in PART B is attached. The statements in PART B are true and correct and have been made by me after personally evaluating the road.

Signature of Civil Engineer

Date

7/27/18



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TRINITY VALLEY  
CONSULTING ENGINEERS, INC

Joshua T. McKnight CE 60687

App# 11963



## ROADWAY EVALUATION

RIVER ACCESS ROAD  
WILLOW CREEK, CALIFORNIA  
APN: 523-021-003

**CLIENT:**  
Eshea Bennett  
Coon Creek Road  
Willow Creek CA 95573

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July 2018  
Josh McKnight, P.E.  
Job #441.06



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## ATTACHMENTS:

- ATTACHMENT 1: LOCATION MAP
- ATTACHMENT 2: SITE MAP
- ATTACHMENT 3: WATER RESOURCE PROTECTION PLAN

## Introduction

Trinity Valley Consulting Engineers was contracted by Ms. Eshea Bennett (owner) to perform an evaluation of the access roadways leading to and throughout the above referenced property. This evaluation is in response to the roadway certification required by the County of Humboldt in association with a cannabis cultivation license application. The following is a summary of the findings, conclusions, and recommendations.

## Project Description

The project at this location is the evaluation of existing roadways used to access the subject parcel. The purpose of this evaluation is to determine whether the roadways are adequate for the intended use. Descriptions of the roadways and their uses are as follows:

**Access Roadways (off property):** These roadways provide access from Horse Linto Road (USFS Road) to the subject property. The roadways have been in place for some time, and were originally constructed and utilized to access timber resources in this area. The property is accessed from one location off of River Road.

**Access Roadways (on property):** Developments within the property are accessed by single lane access roads which appear to have originally been utilized to facilitate logging operations.

## Roadway Inspections

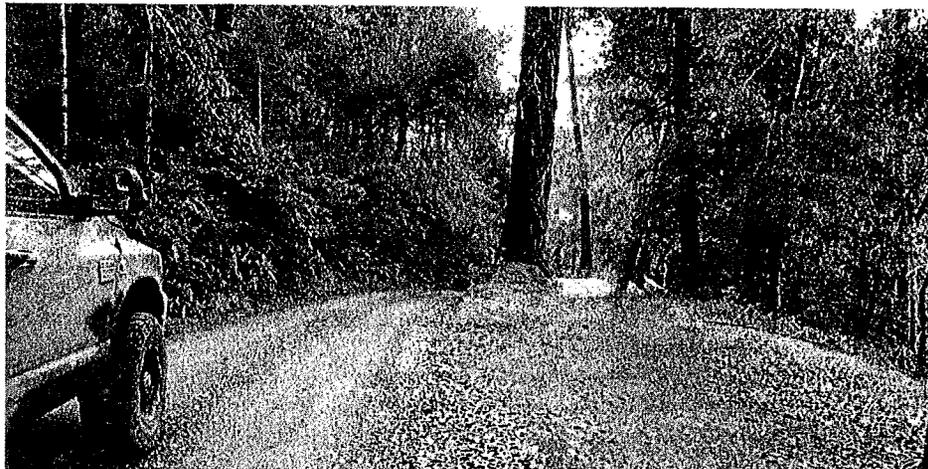
The following is a summary of the observations derived from onsite inspections of these roadways:

**Access Roadways (off property):** Inspection showed these roadways to exhibit the following characteristics:

Roadway Width:	12'-14'
Shoulder Width:	2'
Surface:	Gravel
Slope:	5% to 27%
Drainage:	Rolling dips at approximately every 300'
Watercourse Crossings:	None

Much of this access roadway is on US Forest Service property, with a roadway designation of Forest Route # 7N02. These roadways were inspected and appear to be generally in good shape. Some minor rutting was observed along with one area of limited width. One gate is present along the roadway. While there is not an adequate turnaround area below at the gate, an existing turnaround is was observed approximately one hundred fifty feet downslope of the gate. Adequate turnaround area exists above the gate.





**Typical Roadway**

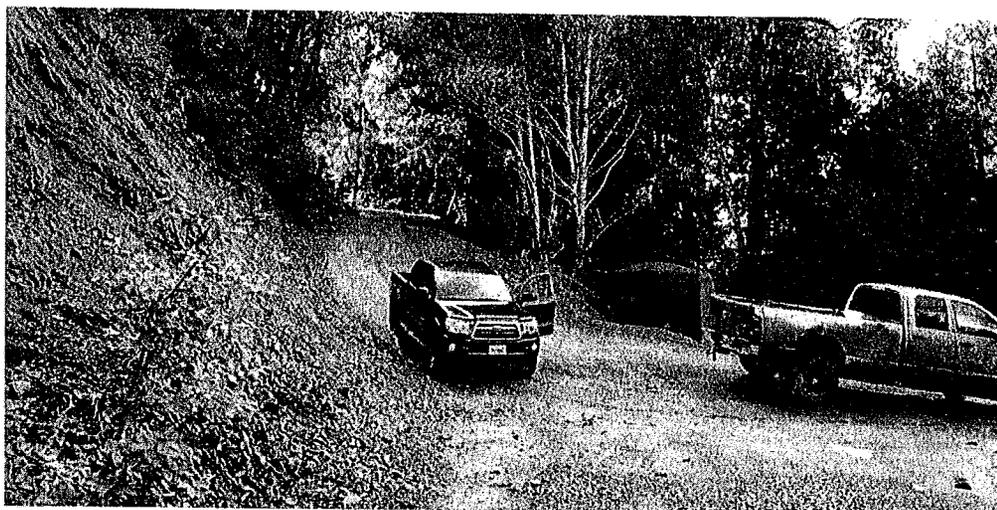
Access Roadways (on property): Access to this roadway is commenced via River Road through a locked gate. Sufficient space was available such that the gate can be unlocked without impacting traffic on Forest Route # 7N02. Inspection showed these roadways to exhibit the following characteristics:

Roadway Width:	10'-12'
Shoulder Width:	1'
Surface:	Gravel
Slope:	5% to 30%
Drainage:	Rolling dips at approximately every 300'
Watercourse Crossings:	None



**Property Access**

These roadways were inspected and are accessible, with some areas of rutting from vehicle traffic. One location exhibited a tight turning radius at an incline. This corner is also the location of a Tee intersection, providing adequate turnaround area. Two seasonal roadways branch off of the main access road for the parcel.



**Tee Intersection**

In addition to the field inspections, observations were made from the Water Resource Protection Plan (WRPP) developed for this property by *Timberland Resource Consultants, October 2016*. Notations in this report denote general roadway conditions, drainage conditions, and potential for discharge to an active watercourse from the roadway runoff.

### **Evaluation**

The following is an evaluation of the above referenced roadways based on the noted observations:

**Access Roadways (off property):** These roadways are in generally good condition. Frequent rolling dips offers good drainage off of the roadway surface while also providing frequent areas for turnouts. Gravel surfacing is adequate with some areas that require minor repairs. While many portions of the roadway exceed grade standards, these areas are also have a straight alignment which can mitigate steep gradients.

**Access Roadways (on property):** These roadways are in adequate condition with areas in need of repairs and maintenance. Roadway widths appear adequate for the use, and steep gradients are on tangent sections of roadway as described above. The corner with the tight radius noted above is mitigated by the Tee intersection at the same location.

**Drainage:** In general all roadways have been constructed to provide adequate drainage. No deficiencies were observed.



**Stormwater Runoff:** Discharge points from the subject roadways do not have potential to enter or adversely affect any watercourses as described in the WRPP.

**Roadway Standards:** The roadways currently do not meet applicable standards due to the steep grades. However, these grades have been mitigated by placing them on tangent sections of roadway making accessibility possible.

**Traffic:** The main access roadway currently serves several parcels. A maximum Average Daily Traffic (ADT) of between 40 and 60 is estimated during peak usage. While this is significant, it is likely far less impact than the traffic impact previously experienced by logging trucks and equipment on this roadway.

### **Conclusion**

The subject roadways are adequate for the intended uses on this property, and the estimated uses for the other properties which they will serve.

### **Recommendations**

The following are recommendations for continued use of these roadways:

**Access Roadways (off property):** Use of these roadways will primarily be a function of continues maintenance. This should include regular grading, addition of rock surfacing when needed, and maintenance of drainage structures.

**Access Roadways (on property):** These roadways will require maintenance as described above as well as the following:

- Regarding roadway to remove ruts and rough areas as well as removal of the outboard berm to allow for sheet flow for roadway runoff.
- Add additional rock surfacing
- Remove vegetation in areas where roadway is potentially impacted
- Construction of new Rolling Dips as specified in the WRPP.

### **Limitations**

This report, recommendations, and conclusions are solely intended for the site discussed above. The information contained in this report is only intended for use at the stated site using the stated uses. This report should not be used as justification for any other project or site, and only be used for information purposes if referenced and reviewed for other projects. TVCE recognize that the site is in a dynamically active area and conditions can and will change. TVCE has used the best professional judgment to assess the present and future risks and assist the landowner in proposing development that does not increase the risk to the resources present in the project area or subject the landowner to untenable hazards. If conditions different from those described in this report are encountered during construction, the project engineer/builder/owner should contact



this office to review the new conditions and evaluate their bearing on the validity of any recommendations provided herein.

The opinions presented herein have been developed using a degree of care and skill ordinarily exercised, under similar circumstances, by reputable civil engineers and geologists practicing in this or similar localities. No other warranty, expressed or implied, is made as to the professional advice included in this report.

The analyses and recommendations contained in this reports are based on the data obtained from subsurface exploration. The methods used indicate subsurface conditions only at the specific locations where soils were observed, and only to the depths penetrated, and cannot always be relied on to accurately reflect stratigraphic heterogeneity that commonly exist between sampling locations.

Do not apply any of this report's conclusions or recommendations if the nature, design, or location of the project changes. If changes are contemplated, the author of this report should be consulted to review the impact on the applicability of the recommendations in this report. The author of this report is not responsible for any claims, damages, or liability associated with any other party's interpretation or the subsurface data or reuse this report for other projects or at other locations without written consent.



**References**

Water Resource Protection Plan  
Timberland Resource Consultants – October 2016





TRINITY VALLEY  
CONSULTING ENGINEERS, INC

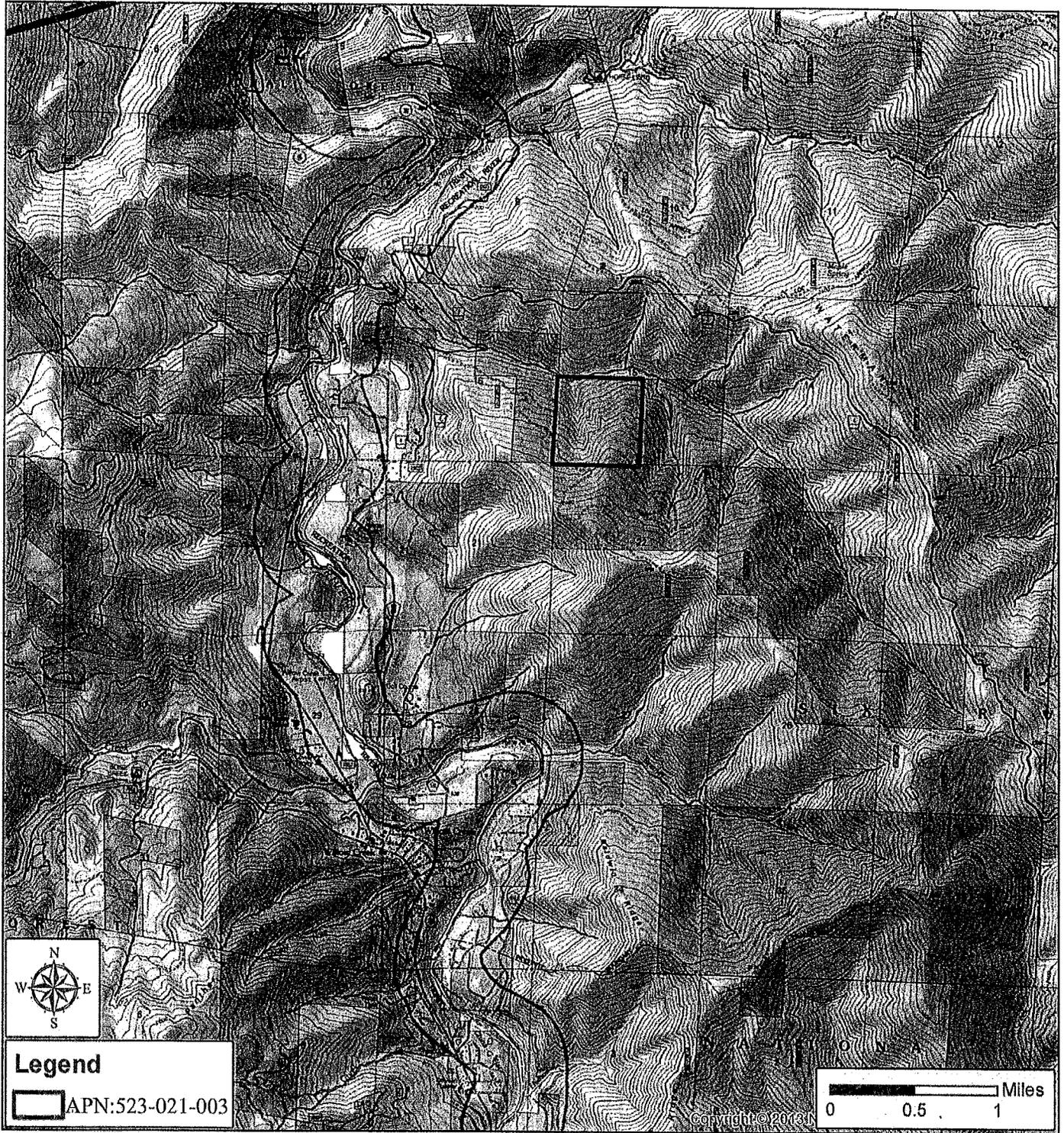
Joshua T. McKnight CE 60687

# ATTACHMENT 1:

## Location Map



Evaluation  
APN: 523-026-004

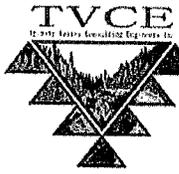


**USGS SEAMLESS TOPOGRAPHIC MAP**  
 FOR  
**COUNTY OF HUMBOLDT**

Parcel Information from Humboldt County GIS  
 Does not reflect exact location of property lines

**Project: Roadway Evaluation**  
 Eshea Bennett  
 River Access Road  
 Willow Creek, California  
 APN:523-021-003

**Location Map**



TRINITY VALLEY  
CONSULTING ENGINEERS, INC

Joshua T. McKnight CE 60687

# ATTACHMENT 2:

## Site Map



Evaluation  
APN: 523-026-004



TRINITY VALLEY  
CONSULTING ENGINEERS, INC

Joshua T. McKnight CE 60687

## ATTACHMENT 3:

# Water Resource Protection Plan



Evaluation  
APN: 523-026-004

WDID# - 1B16469CHUM

# Water Resource Protection Plan



WDID# - 1B16469CHUM

APN 523-021-003

App# 11963

*Prepared by:*

**Timberland Resource Consultants**

**165 South Fortuna Blvd**

**Fortuna, CA 95540**

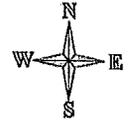
**October 10, 2016**

# Water Resource Protection Plan

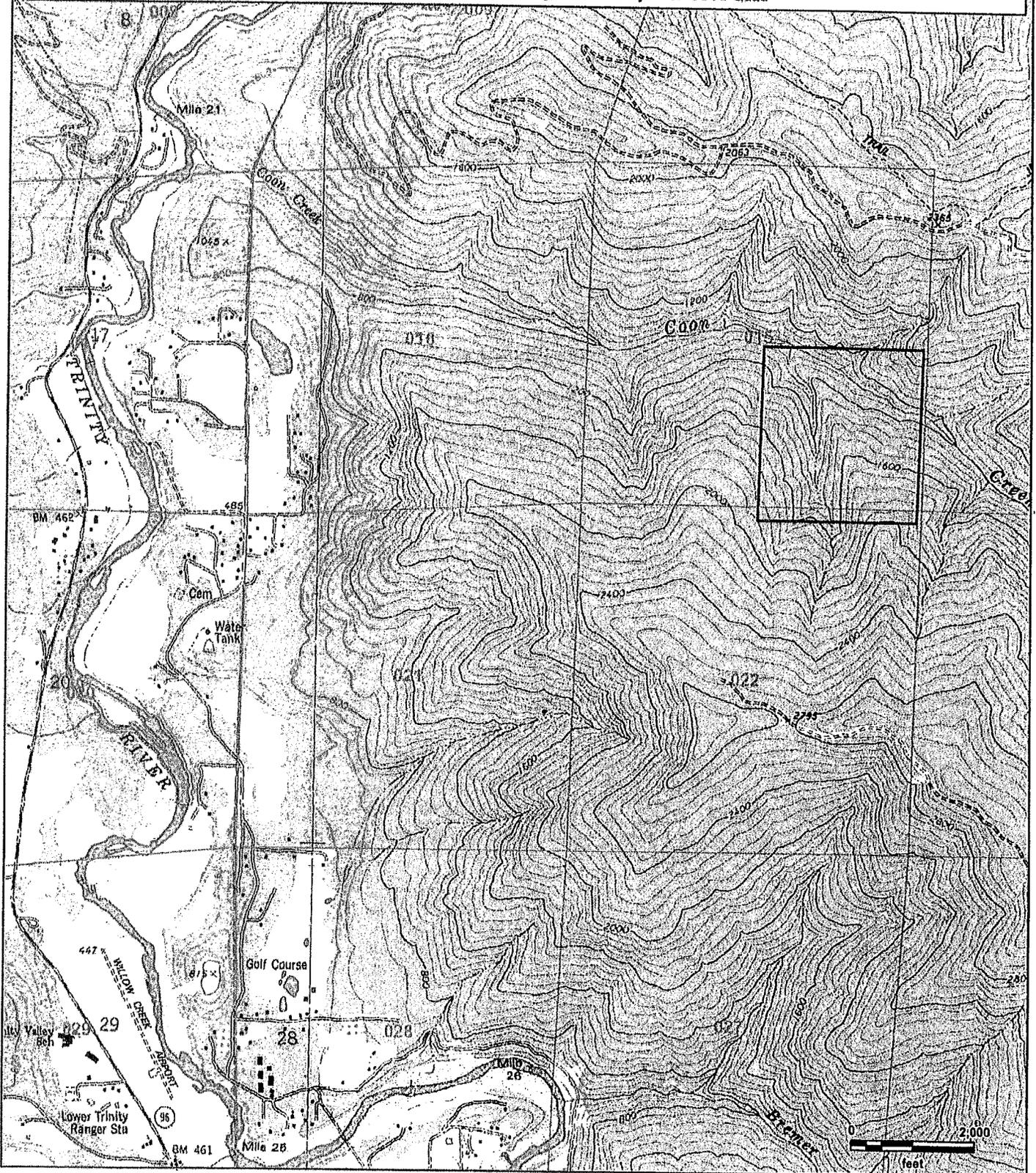
## General Location Map [WDID# - 1B16469CHUM]



 Property Boundary



Section 16, T7N, R5E, HB&M, Humboldt County, from the Salyer 7.5' USGS Quad



**Purpose**

This Water Resource Protection Plan (WRPP) has been prepared on behalf of the discharger, for the property located in Humboldt (APN 523-021-003), by agreement and in response to the California Water Code Section 13260(a), which requires that any person discharging waste or proposing to discharge waste within any region that could affect the quality of the waters of the state, other than into a community sewer system, shall file with the appropriate regional water board a Report of Waste Discharge (ROWD) containing such information and data as may be required by the Regional Water Board. The Regional Water Board may waive the requirements of Water Code section 13260 for specific types of discharges if the waiver is consistent with the Basin Plan and in the public interest. Any waiver is conditional and may be terminated at any time. A waiver should include monitoring requirements to verify the adequacy and effectiveness of the waiver's conditions. Order R1-2015-0023 conditionally waives the requirement to file a ROWD for discharges and associated activities described in finding 4.

**Scope of Report**

Order No. R1-2015-0023 states that "Tier 2 Dischargers and Tier 3 Dischargers who intend to cultivate cannabis before, during, or following site cleanup activities shall develop and implement a water resource protection plan that contains the elements listed and addressed below. Dischargers must keep this plan on site, and produce it upon request by Regional Water Board staff. Management practices shall be properly designed and installed, and assessed periodically for effectiveness. If a management measure is found to be ineffective, the plan must be adapted and implemented to incorporate new or additional management practices to meet standard conditions. Dischargers shall certify annually to the Regional Water Board individually or through an approved third party program that the plan is being implemented and is effectively protecting water quality, and report on progress in implementing site improvements intended to bring the site into compliance with all conditions of this Order." This ownership has been classified as a Tier 2 Discharger under the Order.

**Methods**

The methods used to develop this WRPP include both field and office components. The office component consisted of aerial photography review and interpretation, reviewing of available topographic, geologic and soils mapping, GIS mapping of field data, review of on-site photography points, streamflow calculations, and general planning. The field component included identifying and accurately mapping all watercourses, wet areas, and wetlands located downstream of the cultivation areas, associated facilities, and all appurtenant roads accessing such areas. An accurate location of the Waters of the State is necessary to make an assessment of whether potential and existing erosion sites/pollution sites have the potential to discharge waste to an area that could affect waters of the State (including groundwater). Next, all cultivation areas, associated facilities, and all appurtenant roads accessing such areas were assessed for discharges and related controllable water quality factors from the activities listed in Order R1-2015-0023, Finding 4a-j. The field assessment also included an evaluation and determination of compliance with the Standard Conditions per Provision I.B of Order No. R1-2015-0023. The water resource protection plans required under Tier 2 are meant to describe the specific measures a discharger implements to achieve compliance with standard conditions. Therefore, all required components of the water resource protection plan per Provision I.B of Order No. R1-2015-0023 were physically inspected and evaluated. A comprehensive summary of each Standard Condition as it relates to the subject property is appended.

**Property Description**

The property assessed is a 160 acre ownership located in the SE 1/4 of Section 15, T7N, R5E, HB&M, Humboldt County, from the Salyer 7.5' USGS Quad. The property is located approximately 3 miles north of Willow Creek, California, and is accessed by Horse Linto Creek Road to Forest Route 7N02. Vegetation on the property consists of mixed conifer and deciduous timberland. Elevation is between 1,200' and 1,800' with a predominately north facing aspect. The property contains un-named tributaries and a portion of a Class I watercourse, all of which are tributaries to Trinity River.

**Project Description**

Cultivation on the property consists of five separate sites (Cultivation Areas A-E). Cultivation Areas A (approximately 5,000 ft<sup>2</sup>) & B (approximately 2,000 ft<sup>2</sup>) consists of individual plants in pots on a graded flat. Cultivation Area C consists of an approximately 2,000 ft<sup>2</sup> greenhouse with in-ground beds on a graded flat. Cultivation Area D is approximately 1,800 ft<sup>2</sup> consisting of individual plants in pots on a graded flat. Cultivation Area E is approximately 1,800 ft<sup>2</sup> consisting of a greenhouse with in-ground beds on a graded flat. All cultivation areas are served by a network of seasonal roads.

**Monitoring Plan**

Tier 2 Dischargers shall include a monitoring element in the water resource protection plan that at a minimum provides for periodic inspection of the site, checklist to confirm placement and efficacy of management measures, and document progress on any plan elements subject to a time schedule. Tier 2 Dischargers shall submit an annual report (Appendix C) by March 31 of each year that documents implementation and effectiveness of management measures during the previous year. Tier 2 annual reporting is a function that may be provided through an approved third party program.

Monitoring of the site includes visual inspection and photographic documentation of each feature of interest listed on the site map, with new photographic documentation recorded with any notable changes to the feature of interest. At a minimum, all site features must be monitored annually, to provide the basis for completion of the annual re-certification process. Additionally, sites shall be monitored at the following times to ensure timely identification of changed site conditions and to determine whether implementation of additional management measures is necessary to iteratively prevent, minimize, and mitigate discharges of waste to surface water: 1) just prior to October 15 to evaluate site preparedness for storm events and storm water runoff, 2) following the accumulation of 3" total precipitation or by November 15, whichever is sooner, and 3) following any rainfall event with an intensity of 3" precipitation in 24 hours. Precipitation data can be obtained from the National Weather Service Forecast Office (e.g. by entering the zip code of the parcel location at <http://www.srh.noaa.gov/forecast>).

**Monitoring Plan Reporting Requirements**

Order No. R1-2015-0023, Appendix C must be submitted to the Regional Water Board or approved third party program upon initial enrollment in the Order (NOI) and annually thereafter by March 31. Forms submitted to the Regional Water Board shall be submitted electronically to [northcoast@waterboards.ca.gov](mailto:northcoast@waterboards.ca.gov). If electronic submission is infeasible, hard copies can be submitted to: North Coast Regional Water Quality Control Board, 5550 Skylane Boulevard, Suite A, Santa Rosa, CA 95403.

### Assessment of the Standard Conditions

Assessment of Standard Conditions consisted of field examinations in the spring and fall of 2016. The examination evaluated areas near, and areas with the potential to directly impact, watercourses for sensitive conditions including, but not limited to, existing and proposed roads, skid trails and landings, unstable and erodible watercourse banks, unstable upslope areas, debris, jam potential, inadequate flow capacity, changeable channels, overflow channels, flood prone areas, and riparian zones. Field examinations also evaluated all roads and trails on the property, developed areas, cultivation sites, and any structures and facilities appurtenant to cultivation on the property. Anywhere the Standard Conditions are not met on the property, descriptions of the assessments and the prescribed treatments are outlined following each associated section below.

### Summary of Standard Conditions Compliance

1. Site maintenance, erosion control, and drainage features Y/N
2. Stream crossing maintenance Y/N
3. Riparian and wetland protection and management Y/N
4. Spoils management Y/N
5. Water storage and use Y/N
6. Irrigation runoff Y/N
7. Fertilizers and soil amendments Y/N
8. Pesticides and herbicides? Y/N
9. Petroleum products and other chemicals Y/N
10. Cultivation-related wastes Y/N
11. Refuse and human waste Y/N

#### 1. Site maintenance, erosion control and drainage features

A rocky, permanent road transitions into a seasonal road prior to entering the property. Two seasonal spur roads branch off of this main access road. Seasonal roads are generally in good condition. However, two sites were identified as having inadequate surface drainage and have been addressed in the Mitigation Report below.

Cultivation areas on the property are well maintained, and no existing erosion was identified. Cultivation areas on the property pose no threat to water quality as the closest watercourse to any of the cultivation areas is approximately 410'.

#### 2. Stream Crossing Maintenance

No watercourse crossings were identified during our assessments of the property.

#### 3. Riparian and Wetland Protection and Management

All cultivation related areas, roads, and other potential threats to water quality are at distances beyond those defined as appropriate by the Order.

#### 4. Spoils Management

No construction related spoils were identified during our assessments of the property. If such spoils are created during future construction, they shall be stored in accordance with BMP's in Appendix B of the Order.

**Assessment of the Standard Conditions (Cont.)**

**5. Water Storage and Use**

Currently, water used on this property is sourced from a well on the adjacent property (WDID#1B16468CHUM). The property currently has 18,000 gallons of storage (5 tanks, 1 bladder). The discharger has had a 200,000-gallon rain catchment pond permitted and engineered by Trinity Valley Consulting Engineers that will be constructed in 2017 and fully implement in 2018, at which point the discharger shall use rain catchment water from the pond. Water usage monthly totals associated with cannabis cultivation for 2016 was as follows:

March-	250 gallons
April-	1,500 gallons
May-	6,000 gallons
June-	10,500 gallons
July-	21,000 gallons
August-	21,000 gallons
September-	15,000 gallons
October-	7,500 gallons

**6. Irrigation Runoff**

No irrigation runoff, or evidence of such runoff, was noted during our assessments of the property.

**7. Fertilizers and Soil Amendments**

All fertilizers and soil amendments are stored properly in storage sheds on the property. Products used are applied at rates based on the manufactures specifications. The following is a current list of products stored on site:

**Soil Amendments:**

- Earth Juice - Catalyst
- Canadian Seaweed
- Gypsum
- Epson salt
- Glacier dust
- Dolomite
- Oyster shell

**Fertilizers:**

Seagreen	1-0.5-4
Cal-Mag	2-0-0
Micronutrients	2-1-1
Bonemeal	0-12-0
Potash	0-0-8
Silica	0.5-0.5-1
Mocha Bat Guano	4-6-2
Chicken Manure	3-3-3
Crab Meal	3-4-2
Wegners	8-6-6
Blood Meal	12-0-0
Bone Meal	0-11-0
Biodiversity - SN14	14-0-0

**8. Pesticides/Herbicides**

All pesticides are stored properly in storage sheds on the property. No herbicides are used.

**Pest Controls:**

- Neem Oil
- Nutrillife - SM-90
- Bio Wars
- Dr. Zymes

## Assessment of the Standard Conditions (Cont.)

### 9. Petroleum products and other chemicals

No fuel storage or petroleum products were noted during out assessments of the property.

### 10. Cultivation-related wastes

Organic cultivation related wastes (leaves, branches, stocks, and root wads) were deposited appropriately into a single compost/burn pile between Cultivation Sites D & E on a graded flat approximately 670' from the closest watercourse. Two sites (1 & 4) were identified where cultivation related soils had been stored. Due to the proximity to adjacent watercourses, these sites pose no threat to water quality. Further details and site specific treatments are addressed in the Mitigation Report to follow.

### 11. Refuse and human waste

Garbage and refuse is presently being stored temporarily in garbage cans and regularly hauled to the landfill transfer station. The house on the property contains a restroom that is attached to a permitted septic system.

### 12. Remediation/Cleanup/Restoration

Currently, three of the Standard Conditions are not being met; Site maintenance, erosion control, and drainage features, Water storage and use, and Cultivation-related wastes. Sites will be treated in accordance with regulations, following approval of any and/or all necessary permits, and done in accordance with the BMP's listed in Appendix B of the Order and those included in this WRPP. Additionally, several other general recommendations have been made as follows:

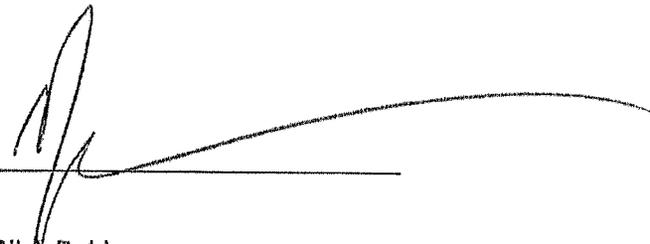
#### General Recommendations

- Collect and store cultivation related wastes tidily in acceptable locations where soil cannot migrate away from the pile by wind or rain and are greater than 100' from a Class II watercourse or greater than 50' from a Class III watercourse.
- Continue to maintain both new and existing road surface drainage features/structures such that any water travelling on the surface of the road cannot travel beyond said drainage features/structures.
- Following the implementation of new and existing road surface drainage structures, it is recommended that remaining rutted road surfaces be graded for improved road surface drainage.
- A water meter shall be installed to record water usage data for the irrigation of cannabis. Any water from the pond used for domestic purposes shall be plumbed separate from that recorded and used for the irrigation of cannabis.

**STATEMENT OF CONTINGENT AND LIMITING CONDITIONS CONCERNING THE PREPARATION AND USE OF WATER RESOURCE PROTECTION PLAN**

**Prepared by Timberland Resource Consultants**

1. This Water Resource Protection Plan has been prepared for the property within APN 523-021-003 in Humboldt at the request of the Client.
2. Timberland Resource Consultants does not assume any liability for the use or misuse of the information in this Water Resource Protection Plan.
3. The information is based upon conditions apparent to Timberland Resource Consultants at the time the inspection was conducted. Changes due to land use activities or environmental factors occurring after this inspection, have not been considered in this Water Resource Protection Plan.
4. Maps, photos, and any other graphical information presented in this report are for illustrative purposes. Their scales are approximate, and they are not to be used for locating and establishing boundary lines.
5. The conditions presented in this Water Resource Protection Plan may differ from those made by others or from changes on the property occurring after the inspection was conducted. Timberland Resource Consultants does not guarantee this work against such differences.
6. Timberland Resource Consultants did not conduct an investigation on a legal survey of the property.
7. Persons using this Water Resource Protection Plan are advised to contact Timberland Resource Consultants prior to such use.
8. Timberland Resource Consultants will not discuss this report or reproduce it for anyone other than the Client named in this report without authorization from the Client.



Nick Robinson  
Timberland Resource Consultants

# Water Resource Protection Plan

## Site Map [WDID# - 1B16469CHUM]

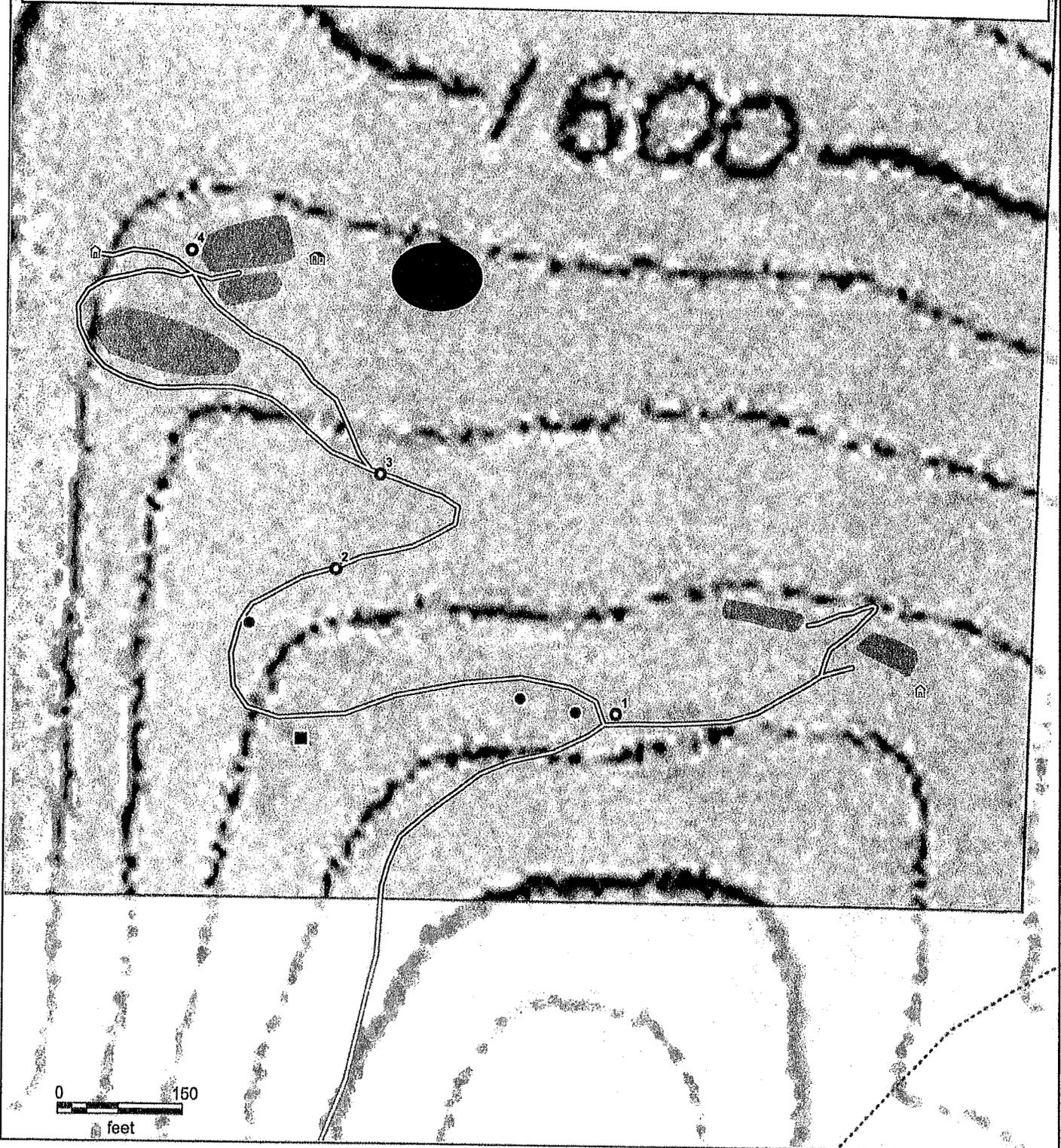
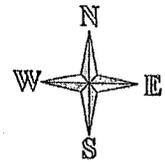


-  Cultivation Area
-  Property Boundary
-  Proposed Pond

-  Site
-  Bladder
-  Tank
-  House
-  Structure

- Watercourses
-  Class I
  -  Class II
  -  Class III

- Roads
-  Permanent
  -  Seasonal



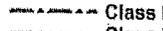
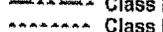
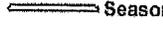
# Water Resource Protection Plan

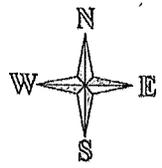
## Site Map [WDID# - 1B16469CHUM]



-  Cultivation Area
-  Property Boundary
-  Proposed Pond

-  Site
-  Bladder
-  Tank
-  House
-  Structure

- Watercourses**
-  Class I
  -  Class II
  -  Class III
- Roads**
-  Permanent
  -  Seasonal





**Timberland  
Resource  
Consultants**

## WRPP - Mitigation Report

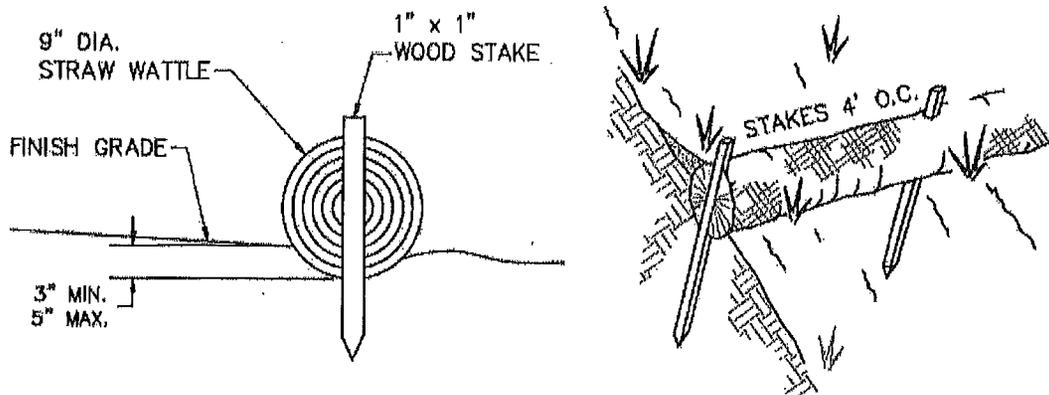
WDID# - 1B16469CHUM

180102111206TRC81

Unique Point	Lat-Long NAD 83	Road Type	Mitigation Planned	Monitor	1600	Standard Conditions	Treatment Priority	Date Completed
1	-123.693543 40.9729	Seasonal	X	X		A.10.	Prior to 10/15/17 pending the approval of any required permits	
<p><b>Current Condition:</b> This location is used for temporary storage of delivered soil. Soil is staged at this site and then transferred to each cultivation area. In this process, some soil has fallen over the edge of the road fill. This site is approximately 1,000' from the nearest watercourse and does not threaten water quality.</p>						<p><b>Prescribed Action:</b> Soil that has gone over the edge shall be seeded with a native seed mix to stabilize it from further transport. Additionally, if this site is to continue to be used for staging soil, an earthen berm, or other solid barrier, shall be constructed along the outboard edge of the road fill to prevent soil from going over the edge in the future.</p>		
Unique Point	Lat-Long NAD 83	Road Type	Mitigation Planned	Monitor	1600	Standard Conditions	Treatment Priority	Date Completed
2	-123.594719 40.973835	Seasonal	X	X		A.1.	Prior to 10/15/17 pending the approval of any required permits	
<p><b>Current Condition:</b> Road lacks adequate surface drainage allowing runoff to concentrate and erode wheel tracks.</p>						<p><b>Prescribed Action:</b> A new type-1 rolling dip shall be installed at this location per the BMP's.</p>		
Unique Point	Lat-Long NAD 83	Road Type	Mitigation Planned	Monitor	1600	Standard Conditions	Treatment Priority	Date Completed
3	-123.694538 40.973631	Seasonal	X	X		A.1.	Prior to 10/15/17 pending the approval of any required permits	
<p><b>Current Condition:</b> Road lacks adequate surface drainage allowing runoff to concentrate and erode wheel tracks.</p>						<p><b>Prescribed Action:</b> A new type-1 rolling dip shall be installed at this location per the BMP's.</p>		
Unique Point	Lat-Long NAD 83	Road Type	Mitigation Planned	Monitor	1600	Standard Conditions	Treatment Priority	Date Completed
4	-123.595338 40.974315	Seasonal	X	X		A.10.	Prior to 10/15/17 pending the approval of any required permits	
<p><b>Current Condition:</b> Existing soil pile located in a cultivation area lacks cover creating the potential for soil to migrate during rain events.</p>						<p><b>Prescribed Action:</b> Soil pile shall be covered during rain events and throughout the winter.</p>		

### **BMP: Erosion Control**

- Erosion control and sediment detention devices and materials shall be incorporated into the cleanup/restoration work design and installed prior to the end of project work and before the beginning of the rainy season. Any continuing, approved project work conducted after October 15 shall have erosion control works completed up-to-date and daily.
- Erosion control materials shall be, at minimum, stored on-site at all times during approved project work between May 1 and October 15.
- Approved project work within the 5-year flood plain shall not begin until all temporary erosion controls (straw bales or silt fences that are effectively keyed-in) are installed downslope of cleanup/restoration activities.
- Non-invasive, non-persistent grass species (e.g., barley grass) may be used for their temporary erosion control benefits to stabilize disturbed slopes and prevent exposure of disturbed soils to rainfall.
- Upon work completion, all exposed soil present in and around the cleanup/restoration sites shall be stabilized within 7 days.
- Soils exposed by cleanup/restoration operations shall be seeded and mulched to prevent sediment runoff and transport.
- Straw Wattles (if used) shall be installed with 18 or 24 inch wood stakes at four feet on center. The ends of adjacent straw wattles shall be abutted to each other snugly or overlapped by six inches. Wattles shall be installed so that the wattle is in firm contact with the ground surface.

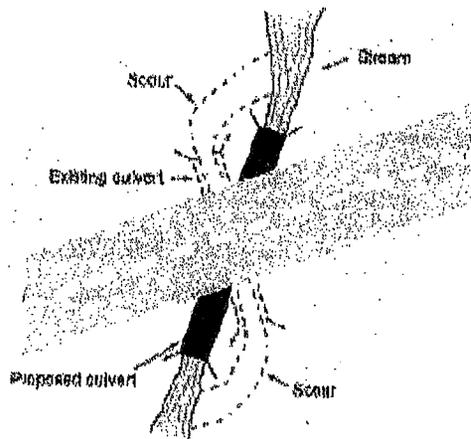


### **BMP: Crossing Abandonment**

- When fills are removed they shall be excavated to form a channel that is as close as feasible to natural watercourse grade, orientation, and a width that is wider than the natural channel.
- Excavated banks shall be laid back to a 2:1 (50%) or natural slope.
- Temporary crossings shall be removed by November 15.
  - Any temporary culvert crossing left in after October 15 or installed between October 15 and May 1, shall be sized to accommodate the estimated 100-year flow.
- Bank and channel armoring may occur when appropriate to provide channel and bank stabilization.
- Road approaches leading to the crossing to be abandoned shall be hydrologically disconnected.
- The abandonment of a watercourse crossing shall not prevent equipment access to existing watercourse crossings not also being abandoned.

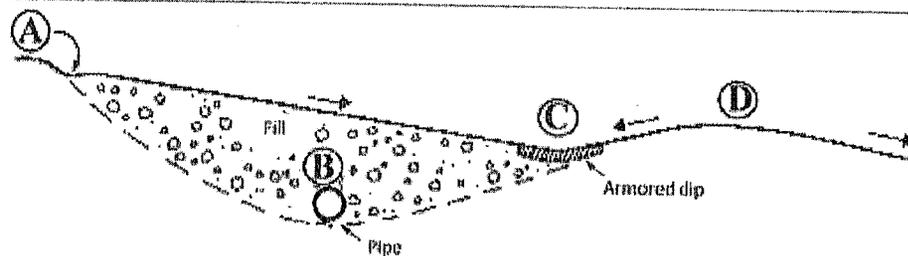
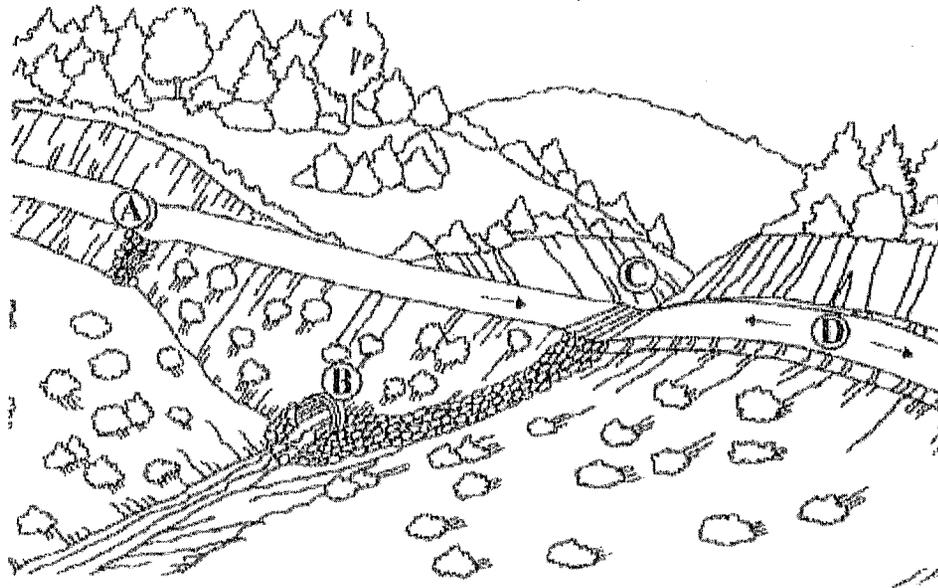
### **BMP: Permanent Culvert Crossing**

- New culvert installations shall be sized to accommodate flows associated with a 100-year storm event.
- If the new culvert is replacing a poorly installed old culvert, the crossing may need to be abandoned to the following standard:
  - When fills are removed they shall be excavated to form a channel that is as close as feasible to natural watercourse grade and orientation, and that is wider than the natural channel.
  - Excavated banks shall be laid back to a 2:1 (50%) or natural slope.
- New culverts shall be placed at stream gradient, or have downspouts, or have energy dissipaters at outfall.
  - Align culverts with the natural stream channel orientation to ensure proper function, prevent bank erosion, and minimize debris plugging. See Figure 97 below.
  - Place culverts at the base of the fill and at the grade of the original streambed or install a downspout past the base of the fill. Downspouts should only be installed if there are no other options.
  - Culverts should be set slightly below the original stream grade so that the water drops several inches as it enters the pipe.
  - Culvert beds should be composed of rock-free soil or gravel, evenly distributed under the length of the pipe.
  - Compact the base and sidewall material before placing the pipe in its bed.
  - Lay the pipe on a well-compacted base. Poor basal compaction will cause settling or deflection in the pipe and can result in separation at a coupling or rupture in the pipe wall.
  - Backfill material should be free of rocks, limbs, or other debris that could dent or puncture the pipe or allow water to seep around the pipe.
  - Cover one end of the culvert pipe, then the other end. Once the ends are secure, cover the center.
  - Tamp and compact backfill material throughout the entire process, using water as necessary for compaction.
  - Backfill compacting will be done in 0.5 – 1.0 foot lifts until 1/3 of the diameter of the culvert has been covered.
  - Push layers of fill over the crossing to achieve the final design road grade, road fill above the culvert should be no less than one-third to one-half the culvert diameter at any point on the drivable surface.
- Critical dips shall be installed on culvert crossings to eliminate diversion potential. Refer to Figure 84 below.
- Road approaches to crossings shall be treated out to the first drainage structure (i.e. waterbar, rolling dip, or hydrologic divide) to prevent transport of sediment.
- Road surfaces and ditches shall be disconnected from streams and stream crossings to the greatest extent feasible. Ditches and road surfaces that cannot be feasible disconnected from streams or stream crossings shall be treated to reduce sediment transport to streams.
- If downspouts are used, they shall be secured to the culvert outlet and shall be secure on fill slopes.
- Culverts shall be long enough so that road fill does not extend or slough past the culvert ends.
- Inlet of culverts, and associate fill, shall be protected with appropriate measures that extend at least as high as the top of the culvert.
- Outlet of culverts shall be armored with rock if road fill sloughing into channel can occur.
- Armor inlets and outlets with rock, or mulch and seed with grass as needed (not all stream crossings need to be armored).
- Where debris loads could endanger the crossing, a debris catchment structure shall be constructed upstream of the culvert inlet.
- Bank and channel armoring may occur, when appropriate, to provide channel and bank stabilization.



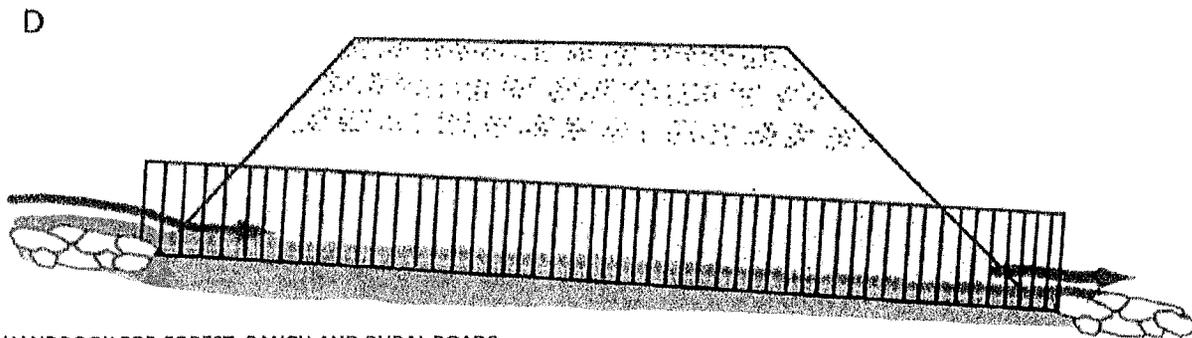
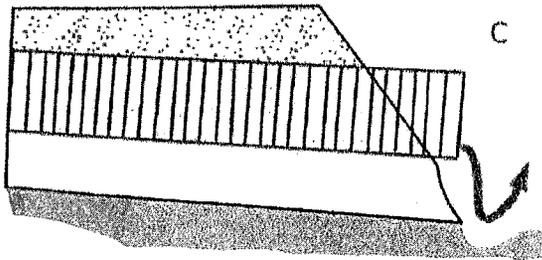
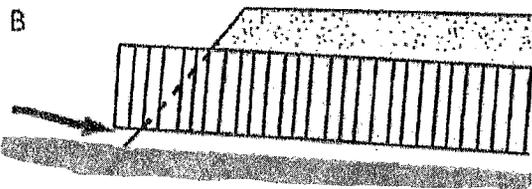
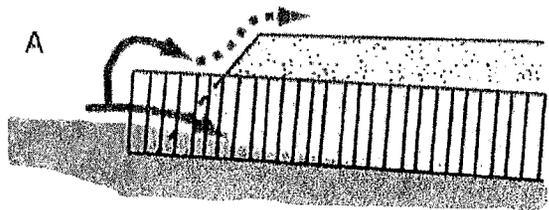
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**FIGURE 97.** *Culvert alignment should be in relation to the stream and not the road. It is important that the stream enters and leaves the culvert in a relatively straight horizontal alignment so streamflow does not have to turn to enter the inlet or discharge into a bank as it exits. This figure shows a redesigned culvert installation that replaces the bending alignment that previously existed. Channel turns at the inlet increase plugging potential because wood going through the turn will not align with the inlet. Similarly, channel turns at the inlet and outlet are often accompanied by scour against the channel banks (Wisconsin Transportation Information Center, 2004).*

**BMP: Permanent Culvert Crossing (Cont.)**

**FIGURE 04.** Critical dips or dipped crossing fills should be centered near a stream crossing's down-road hinge line, not over the centerline of the crossing where overtopping could cause washout or severe erosion of the fill. If the stream crossing culvert (B) plugs, water will pond behind the fill until reaching the critical dip or low point in the crossing (C) and flowing back down into the natural stream channel. The down-road ditch must be plugged to prevent streamflow from diverting down the ditch line. For extra protection in this sketch, riprap armor has been placed at the critical dip outfall and extending downslope to the stream channel. This is only required or suggested on stream crossings where the culvert is highly likely to plug and the crossing fill overtopped. The dip at the hinge line is usually sufficient to limit erosional damage during an overtopping event. Road surface and ditch runoff is disconnected from the stream crossing by installing a rolling dip and ditch relief culvert just up-road from the crossing (A) (Keller and Shtar, 2003).

## BMP: Permanent Culvert Crossing (Cont.)



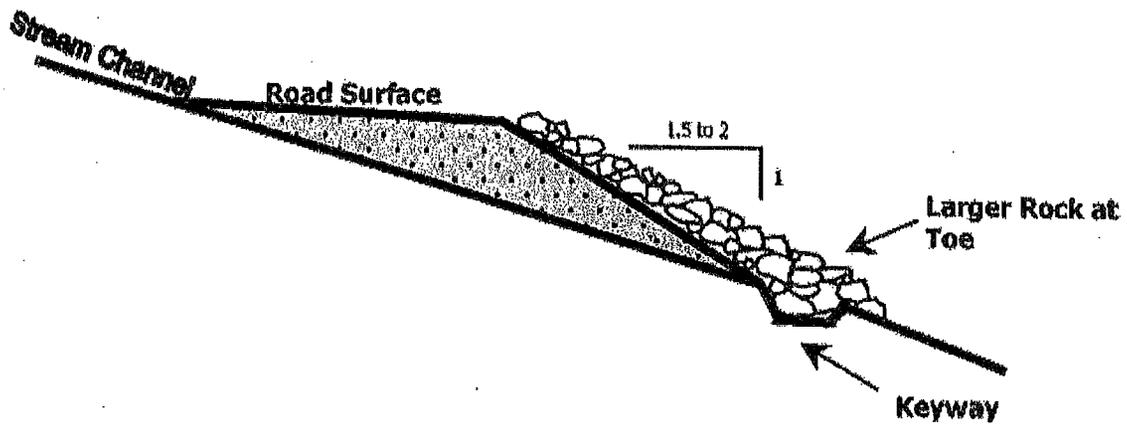
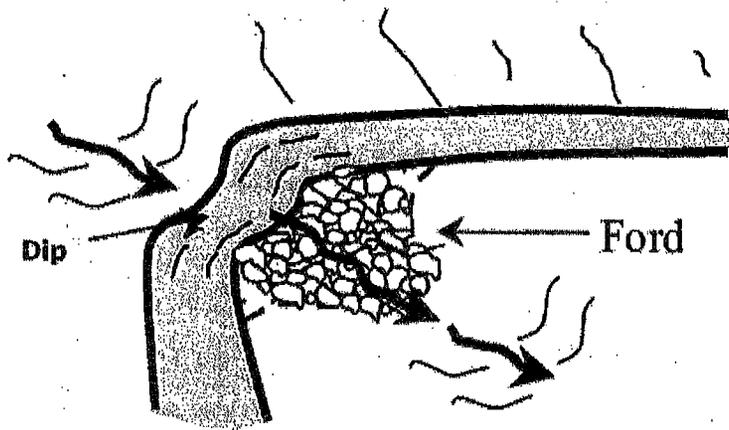
**FIGURE 155.** Proper culvert installation involves correct culvert orientation, setting the pipe slightly below the bed of the original stream, and backfilling and compacting the fill as it is placed over the culvert. Installing the inlet too low in the stream (A) can lead to culvert plugging, yet if set too high (B) flow can undercut the inlet. If the culvert is placed too high in the fill (C), flow at the outfall will erode the fill. Placed correctly (D), the culvert is set slightly below the original stream grade and protected with armor at the inlet and outlet. Culverts installed in fish-bearing stream channels must be inset into the streambed sufficiently (>25% embedded) to have a natural gravel bottom throughout the culvert (Modified from: MDSL, 1991).

### **BMP: Rocked Ford**

- Rocked fords are drainage structures designed to carry watercourses across roads with little to no erosion of the road surface or fill.
- Fords constructed in-channel shall be of appropriately sized material that shall withstand erosion or displacement by expected velocities and placed in a broad, U-shaped channel to create a drivable crossing.
  - The road shall dip into and out of the rocked ford to minimize diversion potential. Construct a broad rolling dip across the roadbed, centered at the crossing, which is large enough to contain the expected 100-yr flood discharge while preventing flood flow from diverting down the road or around the rock armor.
- The road surface at the ford shall be constructed with clean rock. The rock shall be applied to a minimum depth of 6 inches.
  - A range of interlocking rock armor sizes should be selected and sized so that peak flows will not pluck or transport the armor off the roadbed or the sloping fill face of the armored fill.
- The ford's outlet shall be rock armored to resist downcutting and erosion.
  - *Excavate the keyway and armored area* - Excavate a two to three foot deep "bed" into the dipped road surface and adjacent fillslope (to place the rock in) that extends from approximately the middle of the road, across the outer half of the road, and down the outboard road fill to where the base of the fill meets the natural channel. At the base of the fill, excavate a keyway trench extending across the channel bed.
  - *Armor the basal keyway* - Put aside the largest rock armoring to create the buttresses. Use the largest rock armor to fill the basal trench and create a buttress at the base of the fill. This should have a "U" shape to it and it will define the outlet where flow leaves the armored fill and enters the natural channel.
  - *Armor the fill* - Backfill the fill face with the remaining rock armor making sure the final armor is unsorted and well placed, the armor is two coarse-rock layers in thickness, and the armored area on the fill face also has a "U" shape that will accommodate the largest expected flow.
  - *Armor the top of the fill* - Install a second trenched buttress for large rock at the break-in-slope between the outboard road edge and the top of the fill face.
- Road approaches to rocked fords shall be rock surfaced out to the first drainage structure (i.e. waterbar, rolling dip, or hydrologic divide) to prevent transport of sediment using rock.
- Bank and channel armoring may occur when appropriate to provide channel and bank stabilization.
- Road approach rock and rock ford armoring shall be reapplied following use as needed to maintain a permanent crossing.

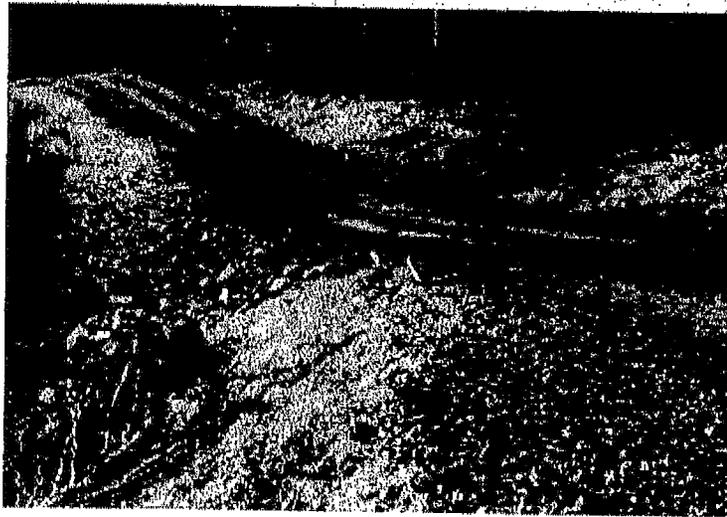
**BMP: Rocked Ford (Cont.)**

**FORD:** A large dip is graded into the road at the axis of the stream channel. The outside fill face is dished out to form a spillway with large rock. On large watercourses, rock is keyed several feet into firm native soils. The road surface is rocked with 6" of minus rock.



### **BMP: Armored Ford (Fill)**

- Armored fords are watercourse crossing fills comprised primarily of rock and designed to carry watercourses across roads without erosion or displacement of installed fill material.
- Armored fords shall have a U-shaped channel to create a drivable crossing.
  - The road shall dip into and out of the armored ford to minimize diversion potential. Construct a broad rolling dip across the roadbed, centered at the crossing, which is large enough to contain the expected 100-yr flood discharge while preventing flood flow from diverting down the road or around the rock armor.
- The road surface at the armored ford shall consist of rock small enough to be easily passable by vehicle, but large enough to not be transported during high flow storm events.
- The ford's inlet shall be rock armored if a threat of head cutting exists.
  - *Excavate the keyway* - Excavate a one to three foot deep "bed" into the inboard edge of the road
  - *Armor the basal keyway* - place various sized rock in the constructed keyway to prevent head cutting. Use the largest rock armor to fill the keyway trench and create a buttress along the inboard edge of the road. This should have a "U" shape to it and it will define the inlet where flow leaves the natural channel and enters the road.
- The ford's outlet shall be rock armored to resist downcutting and erosion.
  - *Excavate the keyway and armored area* - Excavate a two to three foot deep "bed" into the dipped road surface and adjacent fillslope (to place the rock in) that extends from approximately the middle of the road, across the outer half of the road, and down the outboard road fill to where the base of the fill meets the natural channel. At the base of the fill, excavate a keyway trench extending across the channel bed.
  - *Armor the basal keyway* - Put aside the largest rock armoring to create the buttresses. Use the largest rock armor to fill the basal trench and create a buttress at the base of the fill. This should have a "U" shape to it and it will define the outlet where flow leaves the armored fill and enters the natural channel.
  - *Armor the fill* - Backfill the fill face with the remaining rock armor making sure the final armor is unsorted and well placed, the armor is two coarse-rock layers in thickness, and the armored area on the fill face also has a "U" shape that will accommodate the largest expected flow.
  - *Armor the top of the fill* - Install a second trenched buttress for large rock at the break-in-slope between the outboard road edge and the top of the fill face.
- Road approaches to armored fords shall surface rocked out to the first drainage structure (i.e. waterbar, rolling dip, or hydrologic divide) to prevent transport of sediment using rock.
- Bank and channel armoring may occur when appropriate to provide channel and bank stabilization.
- Armored ford armoring shall be reapplied following use as needed to maintain a permanent crossing.



**FIGURE 120.** This armored fill crossing of a steep, ephemeral stream was constructed to provide a low maintenance crossing. The crossing has been deeply dipped to reduce the volume of road fill and to eliminate the potential for stream diversion. The fill slope has been heavily armored through the axis of the crossing to contain flood flows and prevent downcutting. Armored fills cannot be used on fish bearing streams.

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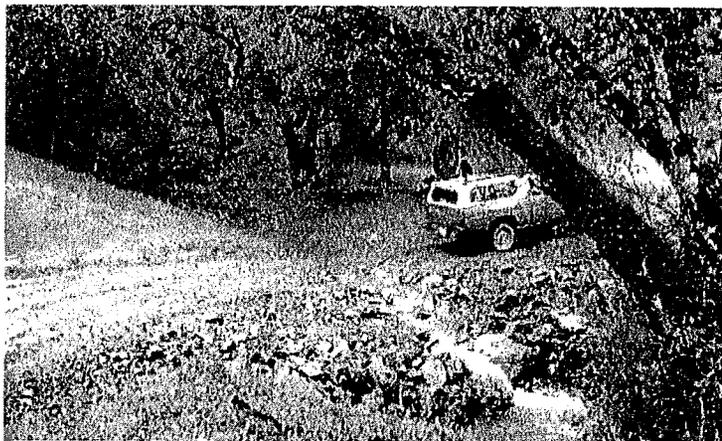
**BMP: Armored Ford [Fill] (Cont.)**



**FIGURE 121D.** Well graded rock armor is then backfilled into the structure and spread across the breadth of the U-shaped stream crossing, and about one-third the way up the roadbed, so that streamflow will only flow over or come in contact with resistant armor material. The armor must be spread and compacted across the design width of the expected flood flow channel width so peak flows will not flank the armored structure.



**FIGURE 121E.** Two weeks after this armored fill was constructed, a storm flow event occurred and the structure maintained its function and integrity. The road approaches had not yet been compacted or surfaced with road rock.



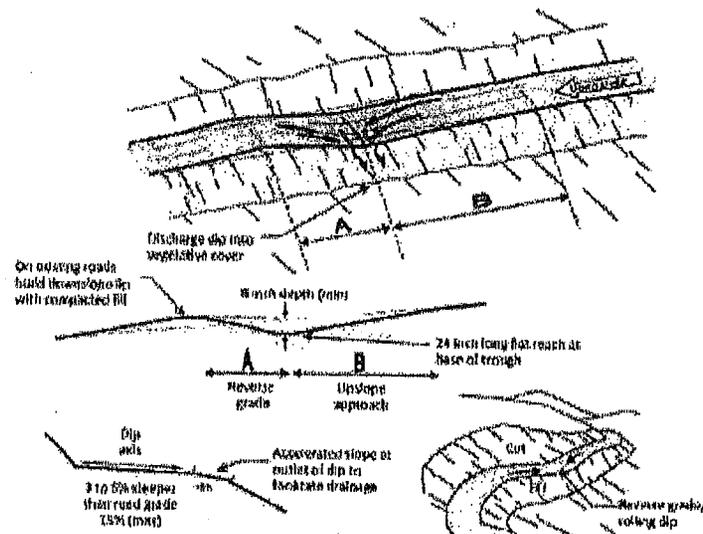
**FIGURE 121F.** The same armored fill as it appeared after the first winter flood flows. No maintenance was required to reopen the road. It is also clear that no stream diversion is possible at this stream crossing site, and the volume of fill within the crossing has been reduced to the minimum amount needed to maintain a relatively smooth driving surface on this low volume road.

### **BMP: Rolling Dip**

- Rolling dips are drainage structures designed to capture and discharge surface water collected on road surfaces and in inside ditches at a specific location.
- The road shall dip into and out of the rolling dip to eliminate the possibility of water flowing along the road surface or in an inside ditch to bypass the dip structure.
- The rolling dip shall be constructed with clean native materials.
- The rolling dips outlet may be armored to resist downcutting and erosion.
- Do not discharge rolling dips into swales that show signs of instability or active landsliding.
- If the rolling dip is designed to divert both road surface and ditch runoff, block the down-road ditch with compacted fill.

### **BMP: Rocked Rolling Dip**

- Rocked Rolling dips are drainage structures designed to capture and discharge surface water collected on road surfaces and in inside ditches at a specific location.
- The road shall dip into and out of the rolling dip to eliminate the possibility of water flowing along the road surface or in an inside ditch to bypass the dip structure.
- The rocked rolling dips inlet and outlet shall be armored to resist downcutting and erosion.
- The entire length of the rocked rolling dip shall be rock armored to a minimum of 5-feet from the centerline of the dip.
- If a keyway is necessary, the rocked rolling dip keyway shall be constructed at the base of the dip and shall be of sufficient size, depth, and length to support materials used in the rocked rolling dip construction back up to the road crossing interface.
- Do not discharge rolling dips into swales that show signs of instability or active landsliding.
- If the rolling dip is designed to divert both road surface and ditch runoff, block the down-road ditch with compacted fill.
- The rolling dip must be drivable and not significantly inhibit traffic and road use.

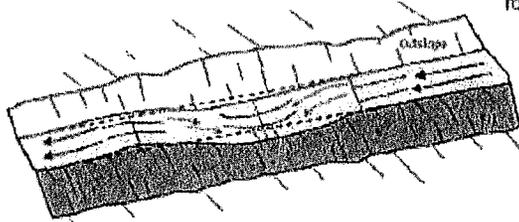


**FIGURE 34.** A classic Type I rolling dip, where the excavated up-road approach (B) to the rolling dip is several percent steeper than the approaching road and extends for 80 to 80 feet to the dip axis. The lower side of the structure reverses grade (A) over approximately 16 feet or more, and then falls down to rejoin the original road grade. The dip must be deep enough that it is not obliterated by normal grading, but not so deep that it is difficult to negotiate or a hazard to normal traffic. The outward cross-slope of the dip axis should be 3% to 5% greater than the up-road grade (B) so it will drain properly. The dip axis should be out-sloped sufficiently to be self-cleaning, without triggering excessive downcutting or sediment deposition in the dip axis (Modified from: Best, 2013).

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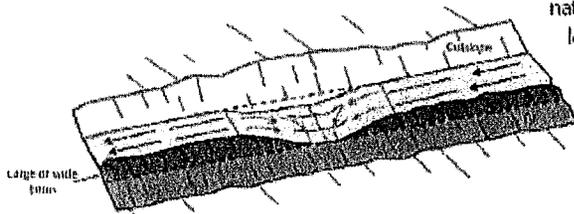
## BMP: Rolling Dip and Rocked Rolling Dip (Cont.)

### Type 1 Rolling Dip (Standard)



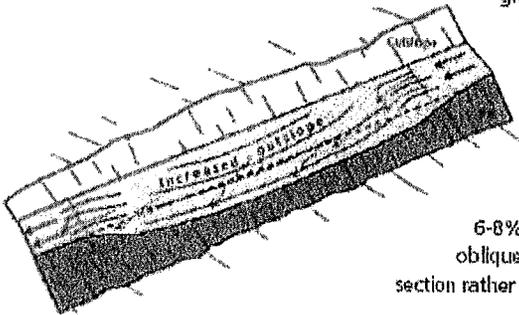
Type 1 rolling dips are used where road grades are less than about 12-14% and road runoff is not confined by a large through cut or berm. The axis of the dip should be perpendicular to the road alignment and sloped at 3-4% across the road tread. Steep roads will have longer and more abrupt dip dimensions to develop reverse grade through the dip axis. The road tread and/or the dip outlet can be rocked to protect against erosion, if needed.

### Type 2 Rolling Dip (Through-cut or thick berm road reaches)



Type 2 rolling dips are constructed on roads up to 12-14% grade where there is a through cut up to 3 feet tall, or a wide or tall berm that otherwise blocks road drainage. The berm or native through cut material should be removed for the length of the dip, or at least through the axis of the dip, to the extent needed to provide for uninterrupted drainage onto the adjacent slope. The berm and slope material can be excavated and endhauled, or the material can be sidecast onto native slopes up to 45%, provided it will not enter a stream.

### Type 3 Rolling Dip (Steep road grade)



Type 3 rolling dips are utilized where road grades are steeper than about 12% and it is not feasible to develop a reverse grade that will also allow passage of the design vehicle (steep road grades require more abrupt grade reversals that some vehicles may not be able to traverse without bottoming out).

Instead of relying on the dip's grade reversal to turn runoff off the roadbed, the road is built with an exaggerated outslope of 6-8% across the dip axis. Road runoff is deflected obliquely across the dip axis and is shed off the outsloped section rather than continuing down the steep road grade.

FIGURE 36. Rolling dip types

### BMP: Steep Road Drainage

- Steep road drainage structures are used on permanent roads where water bars would obstruct access and where roads are too steep (<14%).
- Structures consist of an outsloped section of road leading to a "push-out" ditch that carries water away from the road. The outslope of the road may be either continuous or intermittent at the location of an individual push-out.
- To avoid push-outs requiring regular maintenance, a push-out ditch should have a grade that is greater than or equal to the grade of road draining to it to prevent sediment from settling out and filling in the push-out ditch.

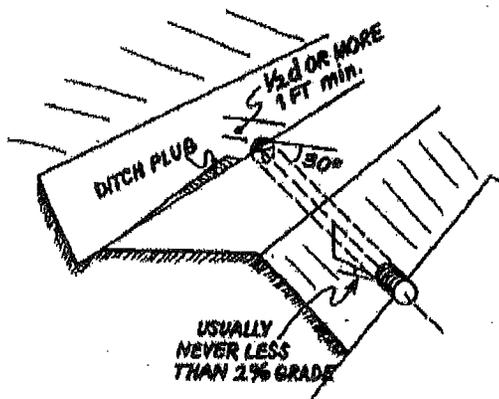


FIGURE 55. Steep roads that go straight up or down a hillside are very difficult to drain. This steep, fall line road developed a through cut cross section that was drained using lead out ditches to direct runoff off the road and onto the adjacent, vegetated hillside. The road was "outsloped" to drain runoff to the right side, and the lead out ditch was built slightly steeper than the road grade, to be self-cleaning. Four lead out ditches have been constructed at 100-foot intervals to the bottom of the hillside.

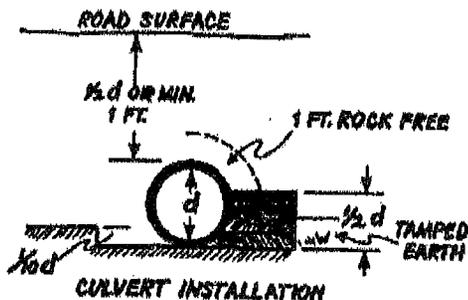
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### **BMP: Ditch Relief Culvert**

- Install ditch relief culverts at an oblique (typically 30 degree) angle to the road so that ditch flow dis not forced to make a sharp angle turn to enter the pipe. On low gradient roads (<5%), where ditch flow is slow, ditch relief culverts can be installed at right angles to the road.
- Install ditch relief culverts (DRC) to outlet at, and drain to, the base of the fill.
- If it cannot be installed at the base of the fill, install the DRC with a grade steeper than the inboard ditch draining to the culvert inlet, and install a downspout on the outlet to carry the culverted flow to the base of the hillslope.
- Downspouts longer than 20 feet should be secured to the hillslope for stability.
- Ditch relief culverts should not carry excessive flow such that downcutting of the ditchline or gullying below the outlet occur.
- Do not discharge flows from ditch relief culverts onto unstable fill or active landslides.
- If the ditch is on an Insloped or crowned road, consider using outsloping to drain the road surface. The ditch and the ditch relief culvert would then convey only spring flow from the cutbanks and hillslope runoff, and not turbid runoff from the road surface.



**FIGURE 48.** The elements of a properly installed ditch relief culvert. The culvert is angled at about 30 degrees to the road alignment to help capture flow and prevent culvert plugging or erosion of the inlet area. It is set at the base of the fill (ideally) or with a grade slightly steeper than the grade of the contributing ditch (but never with a grade less than 2 percent) (USDA-SCS, 1983). At a minimum, the grade of the ditch relief culvert should be sufficient to prevent sediment accumulation at the inlet or deposition within the culvert itself (it should be self-cleaning) (USDA-SCS, 1983).



## BMP: Ditch Relief Culvert (Cont.)

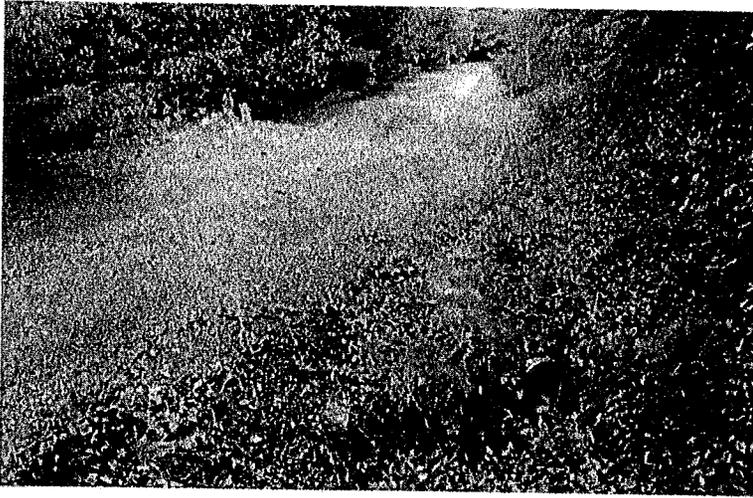
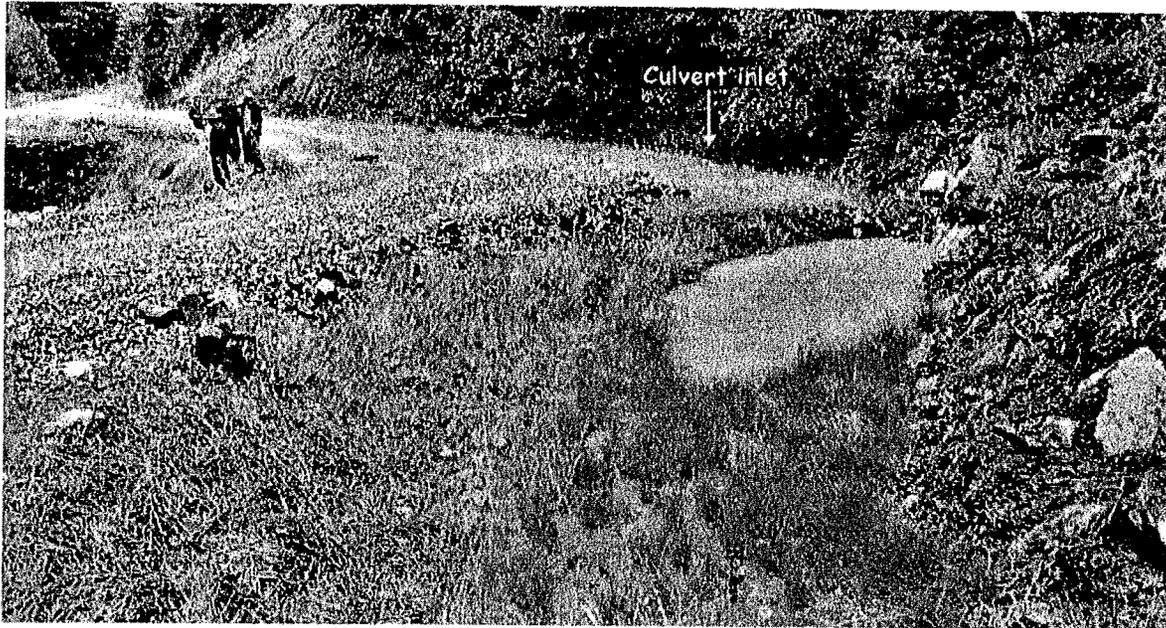


FIGURE 39.

*Waterbars are often used to drain surface runoff from seasonal, unsurfaced roads. Because they are easily broken down by vehicles, waterbars are only used on unsurfaced roads where there is little or no wet weather traffic. In this photo, a waterbar and ditch relief culvert are used to drain all road surface and ditch runoff from the insloped road prism.*

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**FIGURE 238.** Traffic and surface runoff from graveled roads often produces surface erosion, turbid runoff and fine sediment transport that can be delivered to streams. Where ditches can't be eliminated, sediment traps and roadside settling basins can be installed to capture and remove most of the eroded sediment. This settling basin has been constructed along the inside ditch just before a stream crossing culvert inlet (see arrow). Eroded sediment from the road and ditch are deposited in the basin before flow is released to the stream. Fine sediments have filled about 1/3 of this basin and vegetation is now growing. Sediment basins require periodic maintenance to maintain their storage capacity.

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### **BMP: Inlet and Outlet Armoring**

- Inlets of culverts and associate fills shall be protected with rock armoring that extends at least as high as the top of the culvert.
- Outlets of culverts shall be provided a rock energy dissipater at the outfall of the culvert.
- Outlets of culverts and associate fills shall be protected with rock armoring that extends at least as high as the top of the culvert if road fill sloughing into channel can occur.
- Prior to inlet and outlet rocking, the inlet and outlets shall be prepared. Preparation will include removal of vegetation and stored materials from the inlet and outlet.
- Inlets may require construction of an inlet basin.
- Slopes at the outlet should be shaped to a 2:1 or natural slope prior to placing rock armor.
- Rock used at culvert inlets and outlets should be a matrix of various sized rocks and rip-rap that range from a 3" dia. to a 2' dia.
- The largest rocks should be placed at the base of the culvert or fill. Incrementally smaller rocks shall be placed over the larger rocks at the armoring extend up the slope. Voids and spaces shall be back filled with smaller gravels and rocks.

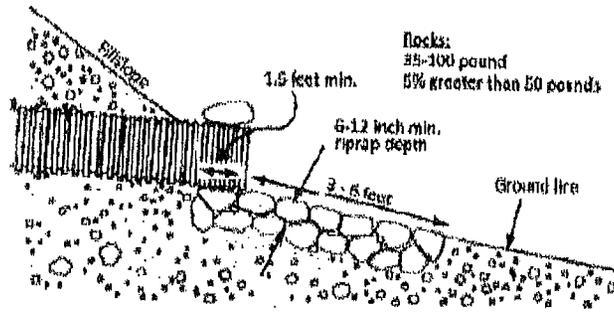


FIGURE 107A. Riprap armor at culvert outlet (Modified from: Kellar et al., 2011).

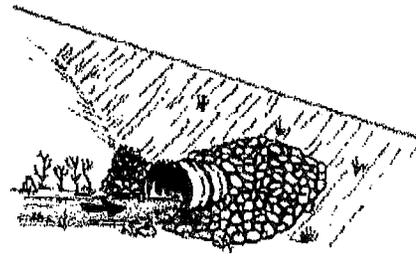


FIGURE 107B. Riprap armor at culvert inlet (Kellar and Sherar, 2003).

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